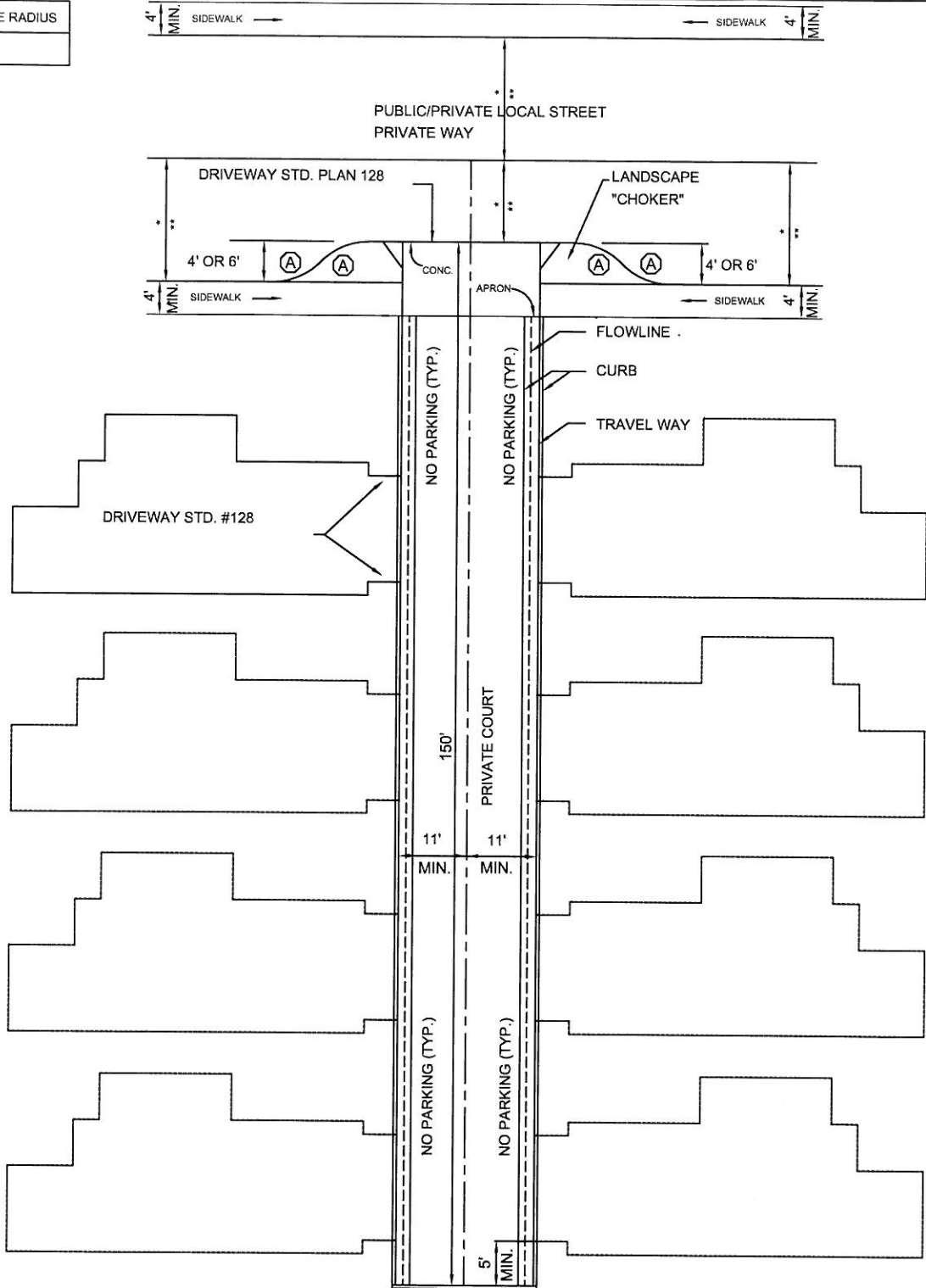


| | |
|-----|--------------|
| | CURVE RADIUS |
| (A) | 25' |



* - PUBLIC/PRIVATE STREET - PAVEMENT WIDTH SHALL BE IN ACCORDANCE WITH STANDARD PLAN 107.

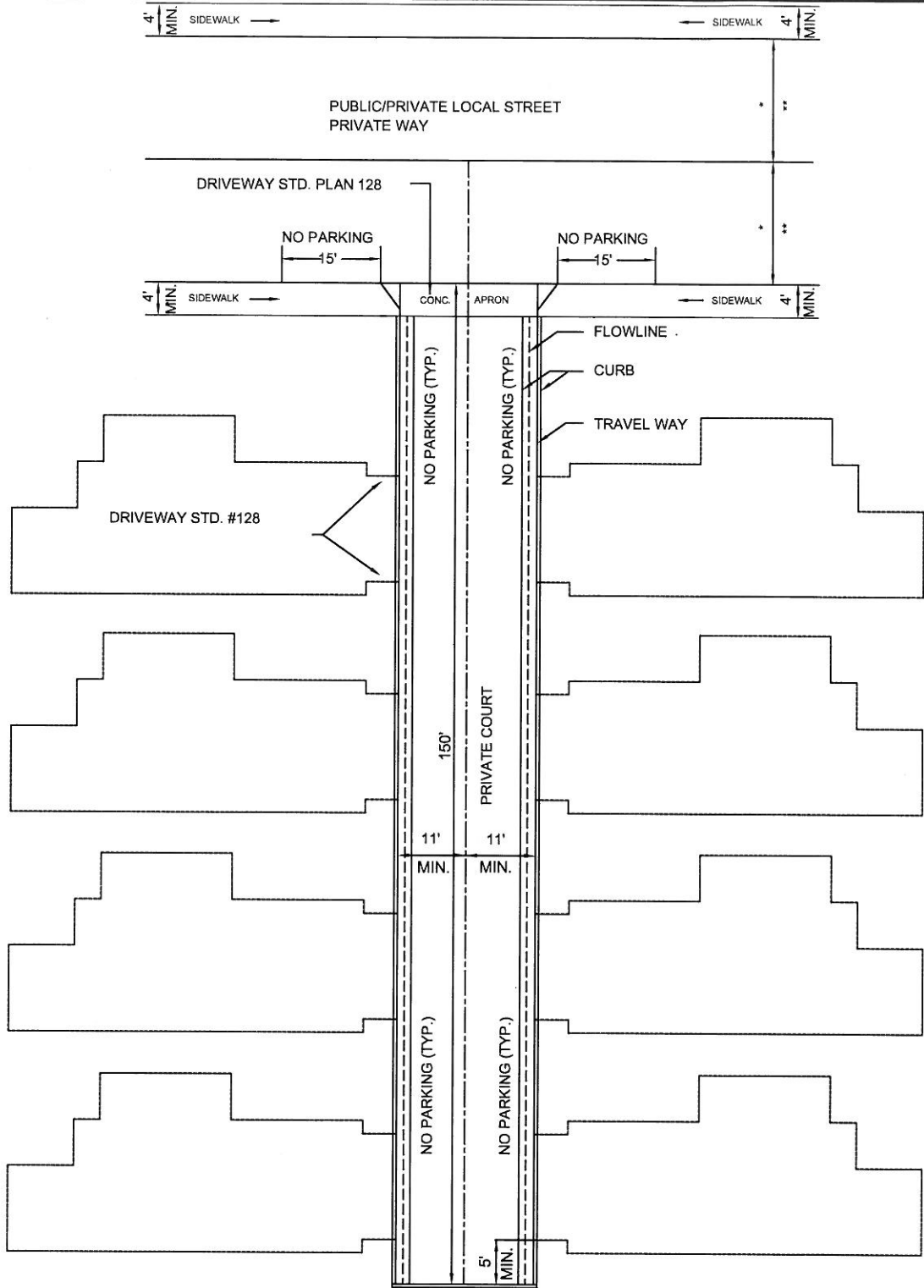
** - PRIVATE WAY - PAVEMENT WIDTH SHALL BE IN ACCORDANCE WITH STANDARD PLAN 111.

WITH CHOKER

| REVISIONS: | | |
|------------|---------|--------------------|
| NO. | DATE | APPRV'D |
| 1 | 4/16/11 | <i>[Signature]</i> |
| | | |
| | | |
| | | |

APPROVED BY:
[Signature] 11-23-09
 CITY ENGINEER DATE
 KIP D. FIELD
 RCE NO. 45679 EXP. DATE 12-31-12

| | |
|---------------------------|------------|
| CITY OF CORONA | |
| PRIVATE COURT | |
| STANDARD PLAN NUMBER: 112 | SHT 1 OF 3 |



* - PUBLIC/PRIVATE STREET - PAVEMENT WIDTH SHALL BE IN ACCORDANCE WITH STANDARD PLAN 107.

** - PRIVATE WAY - PAVEMENT WIDTH SHALL BE IN ACCORDANCE WITH STANDARD PLAN 111.

WITHOUT CHOKER

| REVISIONS: | | | APPROVED BY: <i>Kip D. Field</i> | CITY OF CORONA |
|------------|---------|------------|-------------------------------------|----------------------------------|
| NO. | DATE | APPRV'D | | |
| 1 | 4/16/11 | <i>KDF</i> | 11-23-09 | PRIVATE COURT |
| | | | DATE | |
| | | | CITY ENGINEER KIP D. FIELD | |
| | | | RCE NO. <u>45679</u> | STANDARD PLAN NUMBER: 112 |
| | | | EXP. DATE <u>12-31-12</u> | SHT 2 OF 3 |

DEFINITION

A PRIVATE COURT IS A LOW - SPEED, LOW - VOLUME, DEAD - END TRAVELED-WAY IN RESIDENTIAL NEIGHBORHOODS TO ACCESS RESIDENTIAL GARAGES AND PARKING AREAS. THE MAXIMUM AVERAGE DAILY TRAFFIC ON A PRIVATE COURT SHALL NOT EXCEED 120 TRIPS.

DESIGN CRITERIA

1. SHALL BE DESIGNED TO ACCOMMODATE A MINIMUM 38 FOOT "OUTSIDE" VEHICLE TURNING RADIUS.
2. A MINIMUM CENTERLINE DISTANCE BETWEEN PRIVATE COURTS SHALL BE 75 FEET.
3. THE MAXIMUM DEAD END LENGTH WITH NO TURN AROUND SHALL BE 150 FEET MEASURED FROM THE CURB FACE OF AN INTERSECTING PRIVATE WAY.
4. THE MAXIMUM DEAD END LENGTH WITH TURN AROUND SHALL BE 320 FEET MEASURED FROM THE CURB FACE OF AN INTERSECTING PRIVATE WAY.
5. THE STRUCTURAL SECTION SHALL BE DETERMINED BY A SOILS TEST AND APPROVED BY THE CITY ENGINEER.
6. THE MINIMUM PAVEMENT CROSS SLOPE SHALL BE 1.7%.
7. THE MINIMUM GRADE SHALL BE 1.0% AT THE CENTERLINE UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
8. SEE STANDARD PLAN NUMBER 135 FOR CURB AND GUTTER DETAIL.
9. MINIMUM RADIUS OF 15 FEET SHALL BE USED AT INTERSECTIONS OF PRIVATE COURT TO PRIVATE COURT.

NOTES

1. WHEN THE AVERAGE DAILY TRAFFIC ON THE PRIVATE COURT EXCEEDS 120 TRIPS THE ENTIRE PRIVATE COURT SHALL BE DESIGNATED A PRIVATE WAY AND SHALL BE DESIGNED IN ACCORDANCE WITH CITY STANDARD PLAN 111.
2. THE LATEST ITE MANUAL SHALL BE USED IN DETERMINING AVERAGE DAILY TRAFFIC ON A PRIVATE WAY.
3. THE WIDTH OF A PRIVATE COURT WITHOUT CURB AND GUTTER SHALL BE MEASURED FROM THE EDGE OF PAVEMENT.
4. DRIVEWAYS SERVING ONE RESIDENTIAL UNIT SHALL BE CONSTRUCTED IN CONFORMANCE WITH STANDARD PLAN 128.
5. THE MOUNTABLE CURB SHALL ACCOMMODATE STORMWATER DRAINAGE IN ACCORDANCE WITH CITY STANDARDS.
6. ENHANCED PAVEMENT MAY BE USED ON THE PRIVATE COURT.
7. ALL PEDESTRIAN ACCESS LOCATIONS SHALL CONFORM TO THE LATEST TITLE 24 AND THE AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS.



APPROVED BY: *Kip D. Field* 11/23/09
 CITY ENGINEER DATE
 KIP D. FIELD

CITY OF CORONA

PRIVATE COURT

| REVISION | DESCRIPTION | BY | DATE |
|----------|-------------|----|------|
| | | | |

STANDARD PLAN NUMBER: **112** SHT 3 OF 3