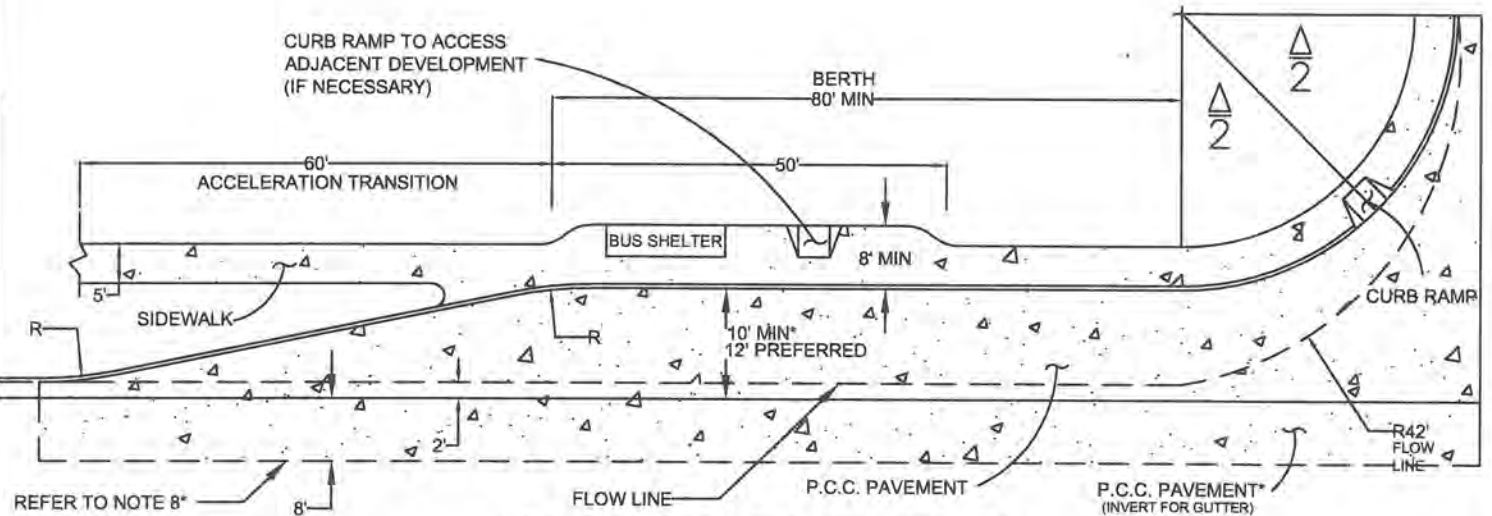


MID-BLOCK TURNOUT



FARSIDE TURNOUT

*4' MINIMUM IF ON-STREET PARKING IS AVAILABLE OTHERWISE 10' MINIMUM

NOTES:

1. R=50'
2. PCC PAVEMENT THICKNESS SHALL BE DETERMINED BY THE ENGINEER OF RECORD OR MINIMUM THICKNESS SHALL BE 8" WITH NO. 3 REBAR AT 18" ON CENTER OF 9" WITHOUT REBAR.
3. BUS SHELTER SHALL BE SETBACK FROM THE FACE OF THE CURB A MINIMUM CLEAR DISTANCE OF FOUR FEET FOR PEDESTRIAN TRAVEL WAY.
4. CURB SHALL BE POURED MONOLITHIC WITH PCC PAVEMENT.
5. MODIFICATIONS OF THIS STANDARD SHALL BE REVIEWED FOR ACCEPTABILITY BY THE PUBLIC WORKS DIRECTOR.
6. CONCRETE SHALL BE CLASS 560C-3250, CURED WITH CURING COMPOUND.
7. ADDITIONAL RIGHT OF WAY MAY BE REQUIRED BY THE PUBLIC WORKS DIRECTOR.
8. DASHED LINE-CONCRETE PAD FOR 4' BUS TURNOUT.

APPROVED BY:

Kip D. Field
CITY ENGINEER
KIP D. FIELD

11-23-09
DATE

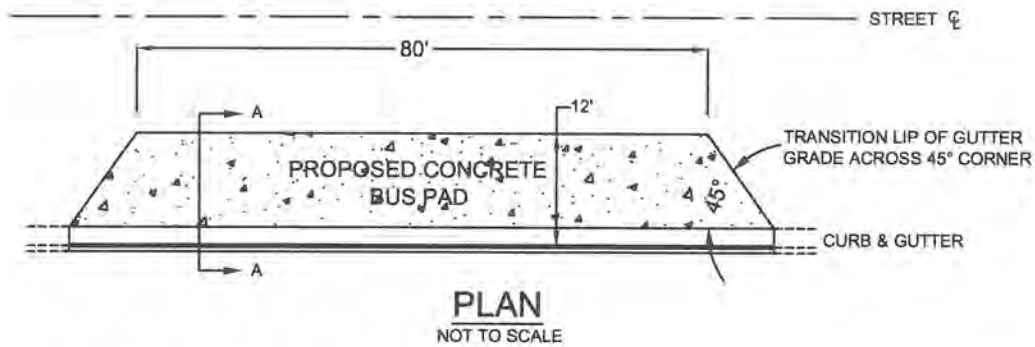
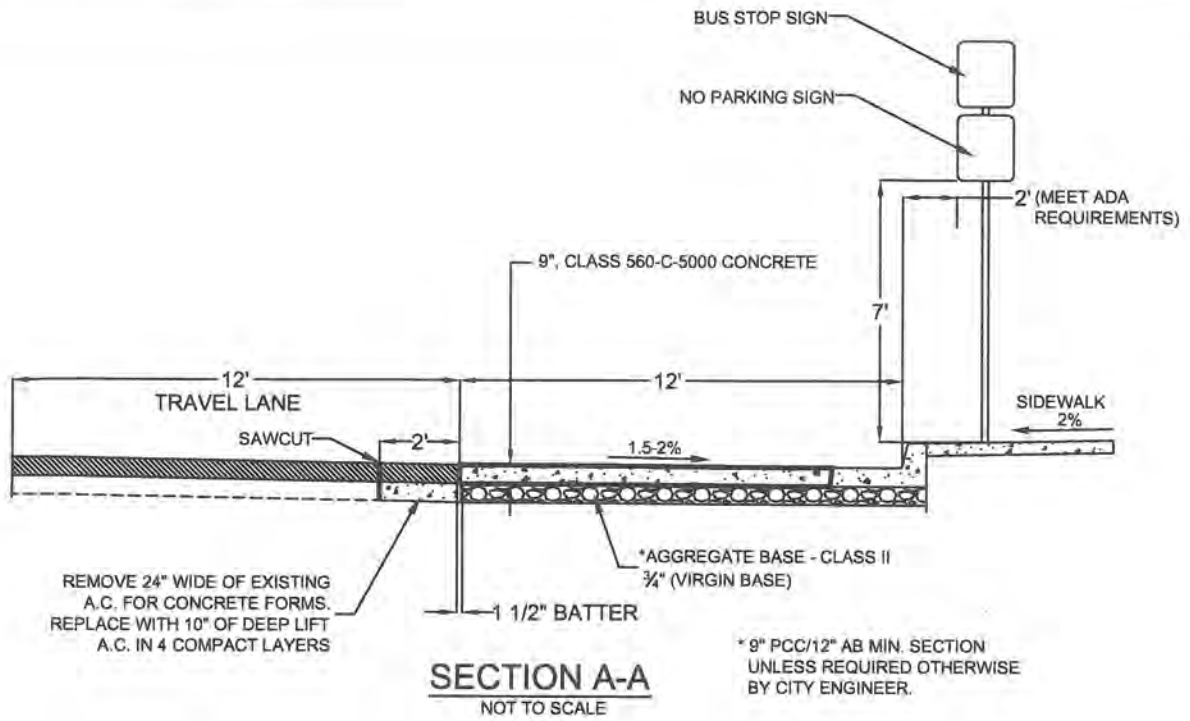
CITY OF CORONA

BUS TURNOUT

REVISION	DESCRIPTION	BY	DATE

STANDARD PLAN NUMBER: 134 SHT 1 OF 5





NOTES:

1. COMPACTION ON THE UPPER 6" OF NATIVE SOIL AND AGGREGATE SHALL BE 95%.
2. CONCRETE CYLINDER TESTS SHALL BE TAKEN AS REQUIRED BY INSPECTOR.
3. CONCRETE SHALL HAVE A MAXIMUM OF 4" SLUMP.
4. ALL CONCRETE TO BE CLASS 560-C-5000.
5. CURING COMPOUND WITH FUGITIVE DYE SHALL BE APPLIED IMMEDIATELY AFTER FINAL FINISHING.
6. 1 1/2" DEEP WEAKENED PLANE JOINTS TO BE INSTALLED AT 20' OC.
5. CONTRACTOR SHALL MAINTAIN TRAFFIC DETOUR, INCLUDING FLASHING ARROW BOARDS FOR A MINIMUM OF 5 DAYS TO ALLOW CONCRETE BUS PAD TO CURE BEFORE PLACING TRAFFIC ON IT.
6. FINAL LOCATION OF BUS PAD TO BE APPROVED IN FIELD BY PUBLIC WORKS INSPECTION PRIOR TO EXCAVATION.
7. CONTRACTOR SHALL FINISH CONCRETE PAD WITH MEDIUM TO HEAVY BROOM FINISH.

APPROVED BY:

Kip D. Field
CITY ENGINEER
KIP D. FIELD

11-23-09
DATE

CITY OF CORONA

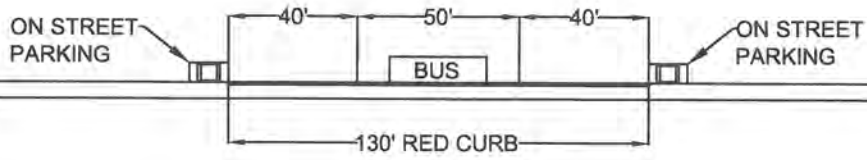
ON-STREET CONCRETE BUS PAD

REVISION	DESCRIPTION	BY	DATE

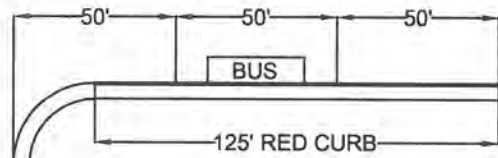
STANDARD PLAN NUMBER: **134** SHT 2 OF 5



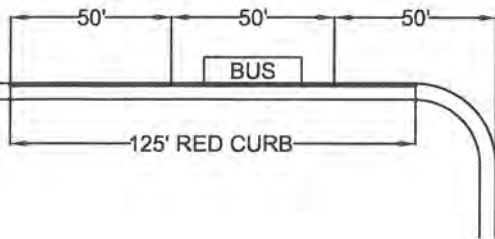
MID-BLOCK STOP



FARSIDE STOP



NEARSIDE STOP



APPROVED BY:
Kip D. Field
 CITY ENGINEER
 KIP D. FIELD

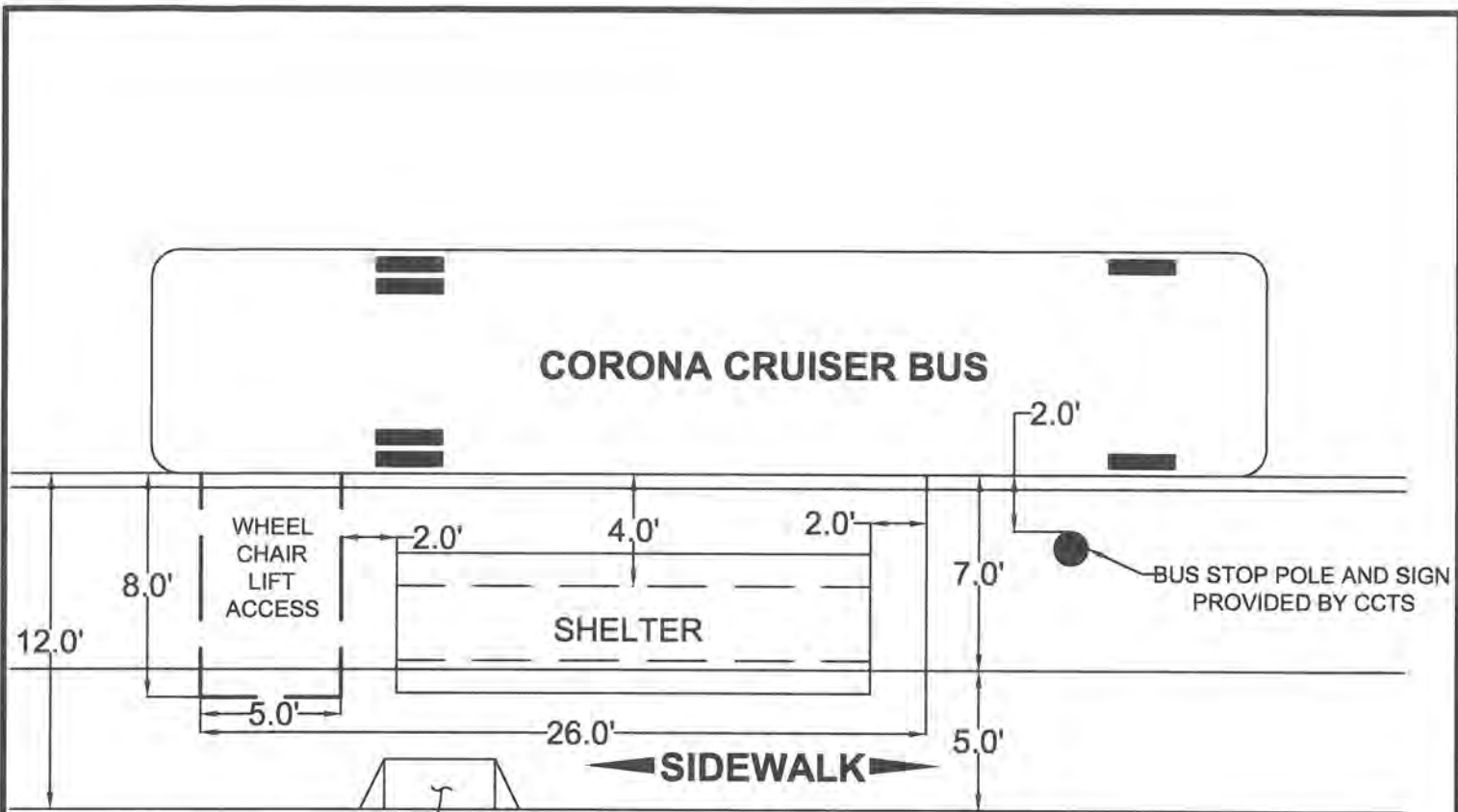
11-23-09
 DATE

CITY OF CORONA

BUS STOP PLACEMENT

REVISION	DESCRIPTION	BY	DATE

STANDARD PLAN NUMBER: **134** SHT 3 OF 5



CURB RAMP TO ACCESS
ADJACENT DEVELOPMENT
(IF NECESSARY)

DEVELOPMENT

NOTES:

1. BUS SHELTER SHALL BE SET BACK FROM THE FACE OF THE CURB A MINIMUM CLEAR DISTANCE OF FOUR FEET FOR PEDESTRIAN TRAVEL WAY.
2. MODIFICATIONS OF THIS STANDARD SHALL BE REVIEWED FOR ACCEPTABILITY BY THE PUBLIC WORKS DIRECTOR.
3. ADDITIONAL RIGHT OF WAY MAY BE REQUIRED BY THE PUBLIC WORKS DIRECTOR.



APPROVED BY:
Kip D. Field
CITY ENGINEER
KIP D. FIELD
11-23-09
DATE

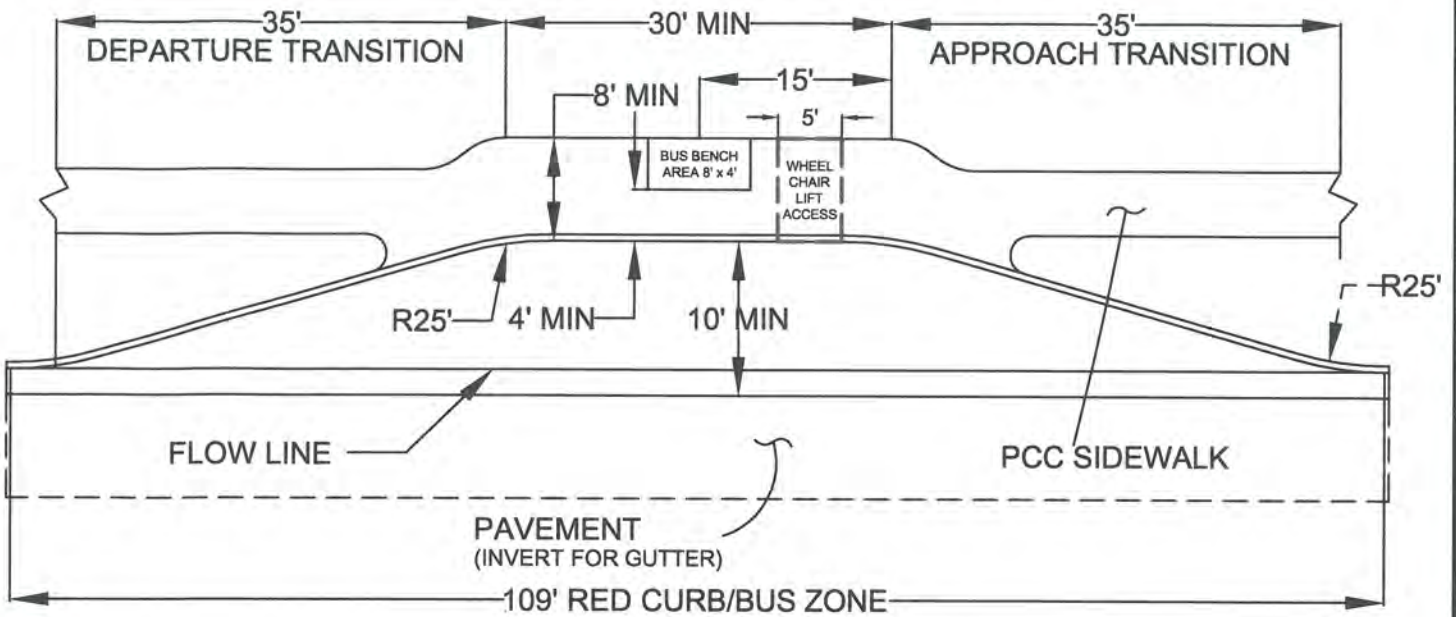
CITY OF CORONA

BUS STOP PLACEMENT

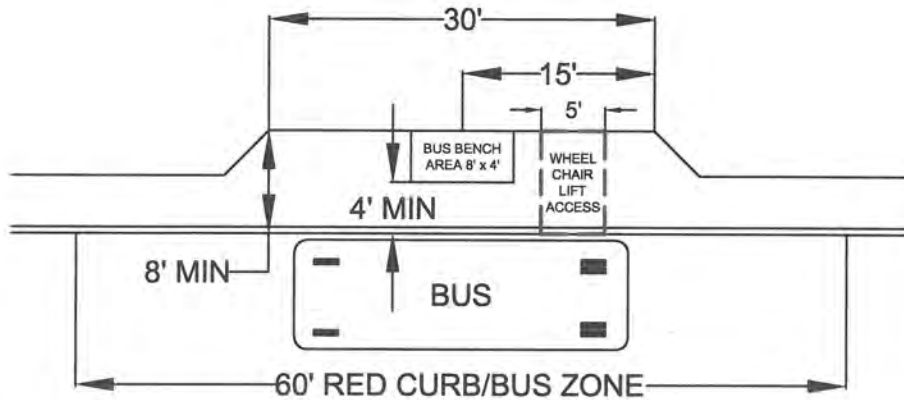
REVISION	DESCRIPTION	BY	DATE

STANDARD PLAN NUMBER: 134 SHT 4 OF 5

NEW PRIVATE DEVELOPMENT BUS STOP TURNOUT



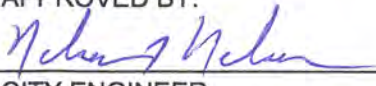
NEW PRIVATE DEVELOPMENT CURBSIDE BUS STOP



NOTES:

1. BUS BENCH SHALL BE SETBACK FROM THE FACE OF THE CURB A MINIMUM CLEAR DISTANCE OF FOUR FEET FOR PEDESTRIAN TRAVEL WAY.
2. PAINTED RED CURB WITH "BUS ZONE" CURB FACE 3" WHITE LETTERING STENCILING SPACED AT 15' INTERVALS.
3. PASSENGER LOADING AREA PCC SHALL BE CLASS 560C-3250, WITH CURING COMPOUND.
4. BUS STOP TURNOUT OR CURB SIDE BUS STOP LOCATION WILL BE REVIEWED AND APPROVED BY THE PUBLIC WORKS DEPARTMENT.
5. PASSENGER LOADING AREA SHALL NOT EXCEED A 2% CROSS SLOPE.
6. ADDITIONAL RIGHT OF WAY MAY BE REQUIRED BY THE PUBLIC WORKS DIRECTOR.

REVISIONS:		
NO.	DATE	APPR'VD

APPROVED BY:

 CITY ENGINEER
 NELSON NELSON

DATE: 1-23-14

RCE NO. 54435

CITY OF CORONA

BUS STOP INSIDE A PRIVATE DEVELOPMENT

STANDARD PLAN NUMBER: 134

SHT 5 OF 5